

LONDON BOROUGH OF ENFIELD

PLANNING COMMITTEE

Date : 28 June 2016

Report of
Assistant Director, Planning,
Highways &
Transportation

Contact Officer:
Andy Higham
Sharon Davidson
Mr Sean Newton

Ward:
Enfield Highway

Ref: 15/01063/FUL

Category: Full Application

LOCATION: Brimsdown Sports Club, Goldsdown Road, EN3 7RP,

PROPOSAL: Part retrospective application for the redevelopment of site to include demolition of existing club houses and ancillary structures, erection of a 2 storey club house with viewing deck incorporating changing rooms, staff and conference room, retention of existing turf pitch, additional stands to main pitch, alteration to ground levels, creation of 2x turf football pitches, 1x full size artificial football pitch and 4x multi use games areas (MUGAs), upgrading of access roads, increase in parking, flood lighting and landscaping.

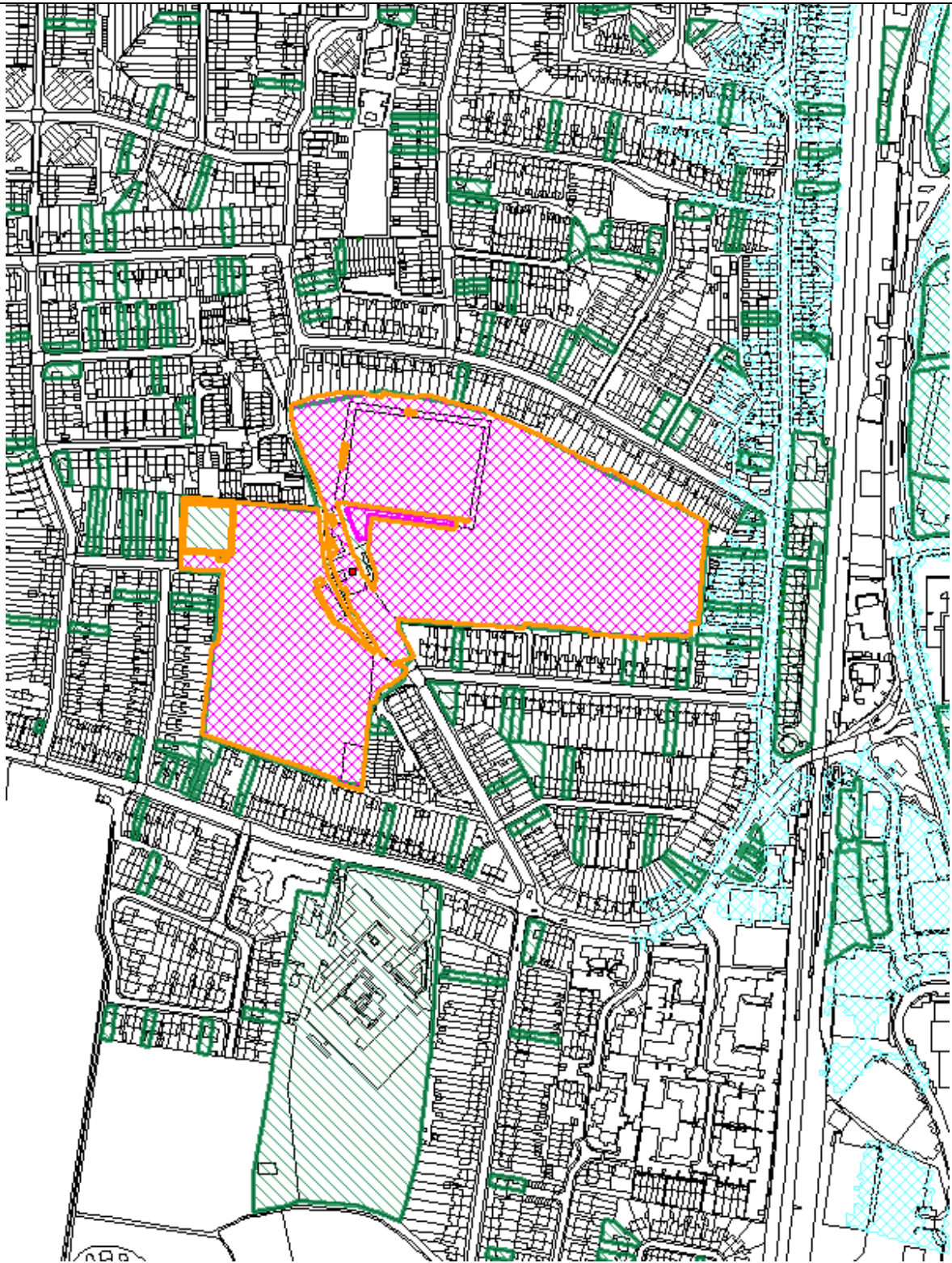
Applicant Name & Address:
Brimsdown Sports Club,
Goldsdown Road
Enfield
EN3 7RP

Agent Name & Address:
Mr Duncan Paterson
106
3 High Street Precinct
Egham
Surrey
TW20 9HN

RECOMMENDATION:

That planning permission is **GRANTED** subject to conditions.

Ref: 15/01063/FUL LOCATION: Brimsdown Sports Club, Goldsdown Road, EN3 7RP,



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Scale 1:5000

North



Site and Surroundings

- 1.1. The site is an existing sports ground located on the northern side of Green Street, accessed off Goldsdown Road.
- 1.2. Between the site entrance and the first of the single storey structures which form a series of clubrooms (Yalova FC and Bush Hill Rangers FC) and ancillary structures is an informal parking area. Immediately to the east of the site entrance and connecting Goldsdown Road with Carterhatch Road to the north, is a public footpath. The footpath bisects the site into an eastern and western side.
- 1.3. The eastern side (also known as "The Stadium Site") is occupied by the aforementioned clubrooms and the main playing pitch, located within the north-west corner. The remainder of this part of the site had been stripped of soil with large stockpiles of imported soil lying in various overgrown bunds.
- 1.4. Around the main pitch are four existing lighting columns and remnants of a covered stand on the northern side of the pitch and an uncovered, seated stand on the southern side. The player dugouts are located on the northern side of the pitch.
- 1.5. The western side (also known as "The Downs") has a single storey clubhouse facing towards a football pitch. Unlike the Stadium Site, the whole of this part of the site would appear to still be in use.
- 1.6. The site is surrounded by residential developments consisting of a mixture of semi-detached, terraced and purpose-built flats on Carterhatch Road to the north, Brimsdown Avenue to the east, Osborne Road to the south and Bowood and Mayfield Roads to the west.
- 1.7. On the southern side of Green Street is Durants Park where there are a number of pitches and MUGA.

2. Amplification of Proposal

- 2.1. Part retrospective application for the redevelopment of site to include demolition of existing club houses and ancillary structures, erection of a 2 storey club house with viewing deck incorporating changing rooms, staff and conference room, retention of existing turf pitch, additional stands to main pitch, alteration to ground levels, creation of 2x turf football pitches, 1x full size artificial football pitch and 4x multi use games areas (MUGAs), upgrading of access roads, increase in parking, flood lighting and landscaping.
- 2.2. Ground levels have been altered through the stripping away of some soil and the unlawful importation of soil. Some of the imported soil will be formed into bunds around the perimeter of the site and some will increase ground levels around the site generally.
- 2.3. The proposed new clubroom facilities will be sited nearer to the main pitch, approximately 40m north of the existing. Facilities to be provided will include:
 - 10 changing rooms
 - Male/female toilet facilities
 - Visitor's lounge bar with kitchen, servery and storage
 - Meeting room / crèche

- Gym
 - Various offices
 - Committee room
 - Conference room
 - Clubroom and bar
 - Partially covered balcony viewing area
- 2.4. Car parking will be provided for 254 vehicles, primarily located to the east of the access road, 14 minibuses and 100 bicycles.
- 2.5. It is anticipated that local schools, clubs and the wider community will have access to the proposed facilities.
- 2.6. Four new spectator stands are proposed around the main pitch. These will include two 30m long touchline stands (approximately 6.5m in height) and two 20m long goal line stands (approximately 6.5m in height).

3. Relevant Planning Decisions

- 3.1. There is an extensive planning history associated with the site. The most relevant are provided below:
- 3.1.1. Outline planning permission (ref: TP/98/1411) was granted with conditions on 21/12/1998 for a new sports pavilion (including changing facilities) and extension to existing changing facilities adjoining sports and social club. The pavilion building was sited on the field to the east of the main clubhouse.
- 3.1.2. Planning permission (ref: TP/95/0824) was granted with conditions on 12/03/1995 for the erection of a bowls club pavilion and changing rooms.
- 3.1.3. Planning permission (ref: TP/89/1352) was granted with conditions on 21/05/1990 for the erection of a bowls club pavilion and changing rooms.
- 3.1.4. Planning permission (ref: TP/06/0441) was granted on 27/04/2006 for an extension to existing seating area to provide 100 additional seats with installation of turnstile.
- 3.1.5. Planning permission (ref: TP/02/0319) was granted on 06/11/2003 for an extension to existing seating area to provide 100 additional seats with installation of turnstile.

4. Consultations

4.1. Statutory and non-statutory consultees

Environment Agency

- 4.1.1. The following has been advised:
- A bespoke environmental permit under the Environmental Permitting Regulations 2010 will be required because there is not currently enough information within the planning application submission to know if the proposed development can meet our requirements to prevent, minimise and/or control pollution and therefore establish whether or not the applicant will be successful in securing a permit for the proposed use.

- No objections are raised subject to the inclusion of various conditions to deal with contamination. This includes a remediation strategy, verification reports, surface water drainage, piling / foundations.

Environmental Health

- 4.1.2. No objections are raised subject to conditions being imposed to secure floodlighting, sound and contamination remediation details.

Sport England

- 4.1.3. It has been advised that there are no objections subject to the imposition of conditions relating to the construction and management of the artificial pitches and hours of use.

Greater London Authority

- 4.1.4. The local planning authority is required to consult with the Mayor's Office where an application falls within one of the categories of potential strategic importance. Consultation with the Mayor's Office, is a two stage process, unless otherwise informed by the Mayor.
- 4.1.5. The stage one consultation response confirms that the Mayor considers that the application does not raise any strategic planning issues and that the application can be determined without further reference to the Mayor.
- 4.1.6. The council is however reminded that electric vehicle charging points should be provided and should therefore be subject to a condition. A construction logistics plan should also be secured by condition. It is requested that a copy of the decision notice and any S106 agreement is sent to the Mayor.

SUDS Officer

- 4.1.7. The following has been advised:

- There are no source control SuDS measures from the runoff from the club house and it's car park. There is therefore unrestricted runoff containing silts from the roof and hardstanding areas that encourage silting of the attenuation tank. This is not acceptable.
- 23 L/s discharge rate off site is acceptable based on the 1.5ha site area
- There has been no rationale behind the type of SuDS utilised in the drainage plan. Again, there is no source control SuDS measures (green roof, rain garden or permeable paving) utilised for the runoff generated by the club house and associated car park. There has been no reason given for the use of the tank as opposed to above ground SuDS measures such as ponds, detention basins etc.
- It was mentioned in the FRA that there is contaminated ground so full infiltration SuDS will not be used. However, partial infiltration can occur, and above ground SuDS (lined swales, basins, ponds) can be utilised.
- There are also no details of levels, cross sections and specifications of the drainage features.
- A detailed management plan is required, outlining the specific actions required to maintain the drainage features

Traffic & Transportation

4.1.8. It has been advised that no objections are being raised.

Tree Officer

4.1.9. It has been advised that there are a number of significant trees located around the boundary of both fields, particularly on the western field. No arboricultural reports have been submitted but it is likely that the construction of the bunds will have a detrimental effect on the root systems of the trees.

4.2. **Public response**

4.2.1. Letters were sent to 359 adjoining and nearby residents in addition to statutory site and press publicity. As a result, 10 letters of objection have been received raising some or all of the following points:

Amenity Issues

- Loss of light to property on Bowood Avenue due to new stands
- Loss of privacy in north-west corner
- Current pitch is higher than gardens, players can look straight into garden
- Ground levels should be taken back to original to maintain privacy
- When matches are currently being played, unable to hold a conversation or hear tv / radio. This happens twice a week.
- Will the area encourage anti-social behaviour?
- Additional pitches with maximum of 88 players plus supporters will increase nuisance throughout week
- PA system currently too loud
- Floodlights installed in 2001 are supposed to be switched off by 22:15. This is contravened on numerous occasions. New lighting should have cut-off timer.
- lights from the playing fields will shine into my home and I have a young son who will need to concentrate on his studies without the distraction of noise and lights.

Highway Issues

- Increase in traffic
- Loss of parking
- Is it possible to have yellow lines on either side of entrance to cul de sac on Goldsdown Road to stop vehicles from blocking the road?

Other matters raised

- Impact on property values because most owners bought into a quiet road / field at the back.
- Will the development impact on potential to sell property?
- Since erection of perimeter fence around the pitch, strip of land between that and houses from No.203 to north-west corner has been neglected and only cleared when previous work started. Used as a dumping ground, attracting vermin. Would like assurance that maintenance will be carried out regularly.
- Cannot tell how mounds relate to floor levels.
- What type of asbestos was found in the soil? Was imported soil tested?
- Will perimeter fence around main pitch be replaced?
- What is the capacity of the new stands?

- Will the floodlights be the same height as the existing?
- Why will light pollution documents only be done after plans agreed?
- What are the hours of proposed use of the lights?
- Will the grounds be secured when not in use?
- Increased danger from flooding
- When will works commence?
- All of the plans in this application show the private access from Mayfield Road ending at the boundary between nos 29 and 27 Mayfield Road. The access road actually ends on the boundary between nos 25 and 23. This may just be an error on the plans but if the perimeter fencing followed the current line shown in red on those plans, it would cut off rear access to numbers 27 and 25 Mayfield Road. This, of course, would be quite unacceptable

5. Relevant Policy

5.1. The London Plan

Policy 3.5	Quality and design of housing developments
Policy 5.1	Climate change mitigation
Policy 5.2	Minimising carbon dioxide emissions
Policy 5.3	Sustainable design and construction
Policy 5.5	Decentralised energy networks
Policy 5.6	Decentralised energy in development proposals
Policy 5.7	Renewable energy
Policy 5.8	Innovative energy technologies
Policy 5.9	Overheating and cooling
Policy 5.10	Urban greening
Policy 5.11	Green roofs and development site environs
Policy 5.13	Sustainable drainage
Policy 5.14	Water quality and wastewater infrastructure
Policy 5.16	Waste net self-sufficiency
Policy 5.18	Construction, excavation and demolition waste
Policy 5.19	Hazardous waste
Policy 5.21	Contaminated land
Policy 6.3	Assessing the effects of development on transport capacity
Policy 6.9	Cycling
Policy 6.12	Road network capacity
Policy 6.13	Parking
Policy 7.1	Building London's neighbourhoods and communities
Policy 7.2	An inclusive environment
Policy 7.3	Designing out crime
Policy 7.4	Local character
Policy 7.6	Architecture
Policy 7.14	Improving air quality
Policy 7.15	Reducing noise and enhancing soundscapes
Policy 7.19	Biodiversity and access to nature

5.2. Core Strategy

CP9:	Supporting community cohesion
CP20:	Sustainable energy use and energy infrastructure
CP21:	Delivering sustainable water supply, drainage and sewerage infrastructure
CP22:	Delivering sustainable waste management
CP24:	The road network

CP25: Pedestrians and cyclists
CP26: Public transport
CP28: Managing flood risk through development
CP30: Maintaining and improving the quality of the built and open environment
CP32: Pollution
CP36: Biodiversity
CP46: Infrastructure contributions

5.3. Development Management Document

DMD37	Achieving High Quality Design-Led Development
DMD38	Design Process
DMD44	Preserving and Enhancing Heritage Assets
DMD45	Parking Standards
DMD47	New Roads, Access and Servicing
DMD48	Transport Assessments
DMD49	Sustainable Design and Construction Statements
DMD50	Environmental Assessment Methods
DMD51	Energy Efficiency Standards
DMD53	Low and Zero Carbon Technology
DMD54	Allowable Solutions
DMD55	Use of Roof Space / Vertical Surfaces
DMD56	Heating and Cooling
DMD57	Responsible Sourcing of Materials
DMD58	Water Efficiency
DMD59	Avoiding and Reducing Flood Risk
DMD60	Assessing Flood Risk
DMD61	Managing Surface Water
DMD64	Pollution Control and Assessment
DMD65	Air Quality
DMD66	Land Contamination and Instability
DMD68	Noise
DMD69	Light Pollution
DMD70	Water Quality
DMD78	Nature Conservation
DMD79	Ecological Enhancements
DMD81	Landscaping

5.4. Other Relevant Policy Considerations

National Planning Policy Framework
National Planning Practice Guidance
LBE S106 SPD
Enfield Characterisation Study
Community Infrastructure Levy Regulations 2010
North East Enfield Area Action Plan (NEEAP) (June 2016)

6. **Analysis**

6.1. **Principle**

- 6.1.1. The principle of the re-use of the site for sporting activity is acceptable having regard to the existing use as a sports ground and the desire at national and local levels to protect and even enhance the provision of open space, sports and recreational facilities.

6.1.2. However, the overall acceptability of the scheme must be assessed against the additional impact from a much greater intensity of use of the site through enlarged clubroom facilities, parking facilities and additional sports pitches.

6.2. **Impact on the Character of the Area**

Design

- 6.2.1. There is clear guidance on the approach to the matter of design. The NPPF (section 7) confirms that the Government attaches great importance to the design of the built environment, with good design being a key aspect of sustainable development but Paragraph 59 of the NPPF confirms that design policies should “*avoid unnecessary prescription or detail and should concentrate on guiding the overall scale, density, massing, height, layout, materials and access of new development in relation to neighbouring buildings and the local area more generally*”. Paragraph 60 further advises that “*decision should not impose architectural styles or particular tastes... [nor] stifle innovation, innovation, originality or initiative through unsubstantiated requirements to conform to certain development forms or styles...[although it is] proper to seek to promote or reinforce local distinctiveness*” while paragraph 61 advises that “*...decisions should address...the integration of new development into the natural, built and historic environment*”.
- 6.2.2. London Plan policies 7.4, 7.5 and 7.6 confirm the requirement for achieving the highest architectural quality, taking into consideration the local context and its contribution to that context. Design should respond to contributing towards “a positive relationship between urban structure and natural landscape features...”. Policy DMD 37 (Achieving High Quality and Design Led Development”) confirms the criteria upon which an application will be assessed. However, it also recognised there is a degree of subjectivity in the assessment of an acceptable design.
- 6.2.3. The proposed clubroom will be significantly larger than the existing as a result of consolidating the existing structures into one building and the desire to provide a facility to serve various other functions (as outlined above at para.2.3). The size of the proposed building is considered appropriate to the application site. Subject to securing appropriate details of the materials, the proposed clubroom will deliver a significant enhancement to the site.
- 6.2.4. The proposed layout is a more intensive use of the site however this is not without precedence, with aerial photographs as recent as 2013 clearly showing five pitches (including the main pitch) on the Stadium Site and one on the Downs Site. It should be noted that only the main pitch was floodlit. The four additional pitches on the Stadium Site are located nearer to the residential properties.
- 6.2.5. A key thrust of planning guidance is to optimise the potential of a site to accommodate development. It is considered that the proposed layout achieves this aim whilst still being sensitive to surrounding residential occupiers by maintaining sufficient distancing to boundaries.

6.3. **Impact on Neighbouring Properties**

Loss of Light / Outlook

Clubroom Facilities

- 6.3.1. The site is surrounded by dwellings fronting Mayfield and Bowood Roads to the west, Carterhatch Road to the north, Brimsdown Avenue to the west, and Osborne and Goldsdown Roads to the south. As the development proposal incorporates works of varying degree / scale over the entire 7.68 hectare site, each adjoining property will potentially be affected to varying degrees.
- 6.3.2. The proposed clubrooms will be significantly larger in terms of footprint and height than the various existing single storey structures. Moreover, it would be positioned approximately 40m north of the existing. At its nearest point, the proposed clubrooms will be sited approximately 15m from the common boundary with No.105 Bowood Road (25m between buildings) but this would be at an acute angle. In addition, replacement trees are proposed along the common boundary, which will help to screen the site.
- 6.3.3. Having regard to all of the above, it is considered that the proposed clubrooms will not unduly impact upon the existing residential amenity of the neighbouring occupiers with regards to loss of light and outlook.

Spectator Stands

- 6.3.4. The nearest dwellings to the west (Bowood Road Nos.107 to 117 odd) of the enlarged stand on the western end of the main pitch are sited between 38m (No.107) and 50m (No.117) distant. At this distance, it is considered that there will not be any detrimental impact on the existing amenity of those adjoining occupiers with regard to loss of light and outlook.
- 6.3.5. The nearest dwelling to the stand on the eastern end of the pitch is approximately 40m away from the nearest dwelling (No.235 Carterhatch Road). Given the level of distancing, being sited south of the properties fronting Carterhatch Road, and with only the 6m deep flank elevation facing those dwellings, there are no concerns with regard to loss of light and outlook.
- 6.3.6. The touchline stand on the northern side of the pitch will be approximately 32m away from the nearest dwellings fronting Carterhatch Road (Nos.211 to 223, odd). Moreover, those dwellings are located north of the site. It is therefore considered that there would be no impact in relation to loss of light and outlook to the aforementioned occupiers.
- 6.3.7. The touchline stand on the southern side of the pitch is approximately 65m away from No.105 Road and will only present its flank elevation towards that property. There are no concerns with regard to loss of light and outlook from this replacement stand.

Overlooking / Loss of Privacy / Distancing

Playing Surfaces

- 6.3.8. The development has involved the importation of a significant amount of soil which at present, are piled into various mounds around the site. Proposed plans show that ground levels will be raised and bunds will be formed around the perimeter of the site.
- 6.3.9. The main pitch will be raised approximately 1m, with the bund to the northern side rising approximately a further 0.5m. Neighbours on Carterhatch Road have advised

that they currently experience some overlooking due the elevated ground level of the main pitch.

- 6.3.10. Observations on site and spot height data would suggest that the existing main pitch is at a similar level to those properties. However, the existing spectator stand near to that northern boundary does include an uncovered section , which when standing at the top level, does offer some views towards those properties (where outbuildings do not prohibit this).
- 6.3.11. The main pitch cannot be lowered because of the need to provide a capping layer as a result of the historic use of the site as an industrial landfill. Whilst the existing situation must be acknowledged, further screening could potentially be provided along the entire length of the northern boundary. This would need to be secured through the submission of a detailed landscaping scheme. It should also be noted that with regard to proposed stands, the details will need to be secured by condition and the design will need to ensure that there is no opportunity to overlook the properties to its rear.
- 6.3.12. The additional playing pitches (including MUGA) would sit approximately 1.5m above ground level to the properties fronting Osborne Road but would be sited between 30m and 40m from the common boundary. At this level of distancing, there should not be any overlooking and loss of privacy, however it is considered that the proposed landscaping could be improved / strengthened along this boundary to further reduce any potential for overlooking, to provide a better visual setting and to contribute to the ecological enhancement of the site.
- 6.3.13. Properties to the east, along Brimsdown Avenue, are sited approximately 100m from the nearest MUGA (70m to their rear boundary) and would be separated by a large area of landscaped bunding. The level of distancing involved together with the bund will not result in undue overlooking and loss of privacy to those occupiers.
- 6.3.14. It should also be noted that the majority of adjoining residential properties have outbuildings at the end of their respective gardens, thus helping to further reduce any opportunity for overlooking and loss privacy.

Clubroom Facilities

- 6.3.15. A viewing deck, recessed into the roof, is proposed, which will wrap around the northern and eastern ends of the building. Due to its design and being primarily eastward facing towards the main pitch and MUGAs, does not raise any concern with regards to overlooking and loss of privacy to neighbouring residential occupiers.
- 6.3.16. At the northern end of the proposed viewing deck, a staircase projects out to provide a level access to the main pitch and down to the changing rooms. This element of the scheme should not lead to any undue overlooking or loss of privacy, however, to further help prevent this, it is considered that additional trees could be provided along the common boundary with those properties fronting Bowood Road. As advised elsewhere, this could be secured through the submission of a detailed landscaping scheme.
- 6.3.17. The proposals indicate that the existing belt of vegetation along the common boundary with properties fronting Bowood Road will be replaced. It is considered that additional trees could be provided, in particular along that part of the boundary to the rear of the main pitch, to further strengthen boundary screening but this would be the subject of a detailed landscaping proposal to be secured by condition.

6.3.18. Rear-facing windows are proposed for the new clubroom however these all consist of rooflights. From cross-sectional drawings, the internal design of the building would not allow for any overlooking from these windows. Moreover, the orientation of the building to the nearest residential building would not lead to overlooking and a loss of privacy.

Noise

6.3.19. Paragraph 123 of the NPPF considers noise impacts of development. It confirms that policies and decisions should aim to:

- avoid noise from giving rise to significant adverse impacts on health and quality of life as a result of new development;
- mitigate and reduce to a minimum other adverse impacts on health and quality of life arising from noise from new development, including through the use of conditions;
- recognise that development will often create some noise and existing businesses wanting to develop in continuance of their business should not have unreasonable restrictions put on them because of changes in nearby land uses since they were established; and
- identify and protect areas of tranquillity which have remained relatively undisturbed by noise and are prized for their recreational and amenity value for this reason.

6.3.20. A Noise Assessment has been provided and gives good consideration to the impact of the proposed development on residents and assesses the impact on the basis of hourly averaged noise levels. The greatest concern associated with such sites is the short-term events, known as Lmax. These short-term events generally cause problems as they are due to shouting during games and the use of PA systems and stand-out well above the general noise levels.

6.3.21. An acoustic bund is proposed, the details of which will need to be agreed in writing. It should be noted that proposed (and implemented) measures will not completely eliminate noise emanating from the use of the site. Regard must be given to the existing situation, which, notwithstanding the greater intensity of use proposed, should not be any worse.

6.3.22. Having regard to the above, it is considered that the development should not unduly impact on the existing amenity of neighbouring occupiers with regard to noise and disturbance. The development is considered to comply with Policy 7.15 of the London Plan, Core Policy 32, Policy DMD68 of the Development Management Plan.

Lighting

Playing surfaces

6.3.23. Given the sensitivities of the site, adjacent to residential dwellings, a lighting scheme should be designed to minimise the impact on these elements (light spillage / light trespass), whilst obviously providing the necessary level of lighting for functional use, and also having regard to existing lighting within the sports ground. For outdoor sporting provision, sports lighting can considerably extend the hours of use especially outside the summer months and is often critical to the viability of many facilities which rely on income from mid-week evening lettings to cover operating costs. Sports lighting is therefore essential if these sports facilities are to be used to their full

capacity and justify the level of capital required to provide them. Without sports lighting, opportunities for sport would be significantly restricted.

6.3.24. The existing site has four lighting columns around the main pitch. The development proposal will include:

- 4no. 15m high lighting columns around the main pitch
- 4no. 15m high lighting columns around the senior artificial pitch
- 18no. 8m high lighting columns around the 7no.5-a-side / MUGA pitches

6.3.25. With regard to the main pitch, although the proposed lighting columns will be taller than the existing (3m) and therefore more noticeable, modern directional floodlighting reduces the amount of light spillage beyond the immediate area. There should be no additional impact from the proposed replacement lighting columns around the main pitch.

6.3.26. Residents towards the eastern end of the site may notice more of an impact from the additional lights proposed for the senior training pitch and the 7no.5-a-side / MUGA pitches as this part of the site is currently undeveloped. However, the nearest facing windows to any of the additional 8m or 15 columns are approximately 56m distant. Retained and proposed trees (some atop bunds) will help screen some of the visual impact. It is considered however, that more could be achieved in terms of plantings but this could be subject to a more detailed landscaping condition.

6.3.27. The submitted Lighting Assessment is considered poor as it does not provide much in the way of detail for the proposed floodlights. The examples contained within it do not match the heights of the masts to be used, which are themselves not uncommon. The only variable should be the specific site circumstances which are dependent upon topography, existing screening and distancing to light sensitive receptors. Notwithstanding the submitted Lighting Assessment, full details of the proposed floodlights and impact from the chosen lights, (e.g. mitigation measures to further reduce spillage, number of lights per column, light hoods, wattage, illuminance plots etc) can be secured by condition. The hours of use for the lighting will also be subject to condition.

6.3.28. No lighting is proposed around the junior pitch in the south-west corner of the site.

Car Parking Areas

6.3.29. Lighting will be required to provide a suitably safe environment for the car parking areas, with the main concern being the extensive parking proposed for the western part of the site. It has been indicated within the Lighting Assessment that low level bollard lighting will be used. This is a type of lighting solution is widely used and considered to be appropriate for light sensitive areas. The parking areas to the east of the retained public footway running through the site will raise no additional concern in terms of impact on neighbouring occupiers from lighting.

6.3.30. Details of the car park lighting scheme will be secured by an appropriately worded condition.

6.3.31. It should be noted that although the main car park will have its ground level raised by approximately 1m, the proposed perimeter bund along the western boundary will approximately be an additional 1.25m in height.

6.4. Traffic and Highway Considerations

Traffic Generation / Parking

- 6.4.1. Comparable sites (i.e. Power League) and associated trip generation data from the TRICS database have been used. The survey sites were chosen on the basis of similar operation, similar PTAL, scale of operation and nature of facilities. This approach is considered acceptable. However the assessment of visitor numbers / movements is weak and assumes a generally low, non-specific amount. The TA considers that the 254 parking spaces provided on site is more than adequate to cater for visitor parking although this excludes the anticipated level of parking / trip generation associated with visitors. The propensity for locally generated trips is good with this location and also considering the Site's extant use. With regard to DMD Policy 48, it is therefore considered that the trip generation is now considered sufficiently robust.
- 6.4.2. Access by pedestrians and cycles will need to be promoted because the site is located east of the A1010 Cycle Enfield proposals. It is therefore proposed that a condition or obligation is included to secure a PERS (Pedestrian Environment Review System) and CLoS (Cycle Level of Service) audit between A1010 and A1055 (including certain side routes, especially connecting to the Site). The Applicant will be required to contribute to identified and agreed improvements.
- 6.4.3. With regard to DMD Policy 45, parking provision should comply with the London Plan. It is noted that the anticipated level of parking is likely to be high given the site's intended use and comparatively low access to public transport (i.e. PTAL 2). The TA concludes that parking provision should be more than adequate for the purposes of the proposed development.
- 6.4.4. Cycle parking appears to be sufficient. All cycle storage will need to be secure, in a location with good natural surveillance and sheltered from the weather.
- 6.4.5. No swept-path analysis has been provided for either the access, parking or footpath crossing areas. The layout proposed appears to broadly accord with general design standards but the details will need to be finalised and secured through conditions or obligations.
- 6.4.6. Parking spaces will need to be reconfigured to suit minibus dimensions without obstructing movement around the site or jeopardising individuals' safety. Active and passive charging points should also be provided in accordance with London Plan and DMD Policy 45 requirements. These elements can be conditioned.

Access

- 6.4.7. The existing access location (in-out) is to remain similar to the existing, although with internal changes to facilitate the movement of pedestrians and cars. The existing footpath is being retained, and will continue on a largely unchanged alignment to the existing. Although it is noted that vehicles are proposed to cross the footpath in order to access the parking areas, it is also proposed that the design, configuration and operation of this interaction point will prioritise pedestrian movement.
- 6.4.8. Although the design appears to broadly accord with current standards, access for emergency services and coaches will need to be more fully demonstrated. As advised, swept path analysis will need to be undertaken and can be secured by condition.

6.4.9. Road Safety Audits will also be required as part of the detailed design, linked to potential s278 works where the proposed designs interact with public highway. This will also need to be secured by condition.

Servicing

6.4.10. The proposed waste collection arrangement broadly complies with current design standards and is therefore considered acceptable, although details will need to be secured by condition / obligation.

6.4.11. The main concern relates to the volume of construction related trips. It is noted that the proposed works and high volume of imported fill provide a longer term environmental over the existing state of the Site. The immediate access points to the Site are currently covered by 5T weight limits although loading access is permitted. Again, the temporary nature of construction traffic is noted.

6.4.12. The TA notes that construction traffic may be focused on times outside the operation of nearby schools – notably Brimsdown Primary School – this is supported. The proposed construction routing is considered acceptable, although the introduction of a residents' / school / business consultation group is very strongly recommended.

6.4.13. A car park management plan would be beneficial in order to minimise the risk of overspill parking to the surrounding residential streets where parking pressures can, and have been observed to, be high.

6.4.14. A Construction Management Plan has not been provided although it is acknowledged that reference is made to providing one should the development be approved. This will be secured by an appropriately worded condition..

6.4.15. Having regard to the above, it is considered that the development provides acceptable parking and servicing facilities having regard to Policy 6.13 of the London Plan, and Policy DMD45 of the Development Management Document.

6.5. Sustainable Design & Construction

Energy Efficiency

6.5.1. The London Plan confirms that non-domestic buildings in the period 2013-2016 should be looking to achieve a 35% improvement on 2013 Building Regulations. A detailed Energy Strategy will need to be provided to clearly set out how the scheme will achieve (or exceed) the target savings. This information will be required prior to development commencing because the overall appearance of the building can be detrimentally affected, depending upon the measures proposed.

Biodiversity / Ecology

6.5.2. Policy 7.19 of the London Plan (“Biodiversity and access to nature”) requires development proposals to make a positive contribution, where possible, to the protection, enhancement, creation and management of biodiversity. Core Policy 36 of the Core Strategy confirms that all developments should be seeking to protect, restore, and enhance sites. Policy DMD79 advises that on-site ecological enhancements should be made where a development proposes more than 100sqm of floor space, subject to viability and feasibility.

- 6.5.3. The western sector of the site is predominantly playing fields in current active use. The eastern sector generally comprises land stripped of soil (now grown over), with stockpiles of stripped material, along with a car park, stadium in current active use and associated club house and other ancillary buildings. The two sectors of the Site are variably bordered by hedgerow / tree belt and other peripheral vegetation
- 6.5.4. The initial appraisal indicates that whilst the majority of the site generally comprises playing fields or cleared land of minimal ecological interest, there are specific areas of mainly peripheral habitat to be retained, which could provide suitable habitat for a number of statutorily protected and other notable species eg. badgers and reptiles. There could also be a possible presence of roosting bats in buildings and/or a small number of trees within and close to the site. Parts of the site are also suitable for use by breeding birds. These possibilities have been assessed within the submitted Report and it has been established that:
- there is no evidence of badgers;
 - there is no suitable habitat for Great Crested Newts;
 - some parts of the site (identified on the “Habitat Zonation Drawing”) could have the potential to provide habitat for the slow worm, grass snake and common lizard, therefore a herpetological survey may be required should development occur in these areas.
 - None of the existing buildings provide suitable habitat for bats. Some of the existing trees have the potential (albeit, low) to provide roosts.
- 6.5.5. Whilst some perimeter trees and hedgerow will be retained, a substantial number will be removed to enable the redevelopment of the site. Vegetation clearance should only be permitted outside of the bird nesting season or if unavoidable within the nesting season, only under the supervision of an appropriately qualified ecologist. Should planning permission be granted, a condition could be reasonably imposed to secure this.
- 6.5.6. With regard to enhancements, the Ecological Appraisal recommends that a Nature Conservation Management Plan is produced for all new and retained habitat, to include replacement planting as necessary, establishment maintenance, and a management strategy / monitoring. This also includes the provision of bird and bat boxes.
- 6.5.7. Notwithstanding the above, there has been no discussion on the provision of a biodiverse / green roof and living walls, which would also contribute to flood risk alleviation. There are green roof systems which would enable their provision on pitched roofs. The issue is whether this will be feasible at all but to not have any regard is considered unacceptable and contrary to DMD59, 61 & DMD79.
- 6.5.8. The recommendations contained within the Ecological Appraisal will be conditioned. In addition, a further condition will be imposed to investigate the feasibility of providing a biodiverse / green roof. As discussed above, a lighting condition will be imposed to ensure that any proposed lighting is sensitive to the surrounding environment. From an ecological perspective, this will include wildlife habitat.

Trees / Landscaping

- 6.5.9. An Arboricultural Assessment has not been provided. As currently proposed, the toe of the bund will be in close proximity to some retained trees. Increasing ground levels around the base of a tree can potentially harm its long-term life expectancy, therefore

it is recommended that where the bund finishes in close proximity to an existing tree, as a minimum it should be no nearer than the outer extent of the root protection area of the relevant tree. Conditions are proposed to secure a revised detailed layout plan, an arboricultural constraints plan, and a tree protection plan.

- 6.5.10. Having regard to the above, it is considered that the development proposals should not unreasonably impact on the health of retained trees, and where existing planting will be removed, sufficient replacement planting is proposed.

Drainage

- 6.5.11. London Plan policies 5.12 and 5.13 requires the consideration of the effects of development on flood risk and sustainable drainage respectively. Core Policy 28 (“Managing flood risk through development”) confirms the Council’s approach to flood risk, inclusive of the requirement for SuDS in all developments. Policies DMD59 (“Avoiding and reducing flood risk”) confirms that new development must avoid and reduce the risk of flooding, and not increase the risks elsewhere and that Planning permission will only be granted for proposals which have addressed all sources of flood risk and would not be subject to, or result in unacceptable levels of flood risk on site or increase the level of flood risk to third parties. DMD61 (“Managing surface water”) requires the submission of a drainage strategy that incorporates an appropriate SuDS scheme and appropriate greenfield runoff rates.
- 6.5.12. A Flood Risk Assessment / Sustainable Drainage Strategy has been submitted in support of the application. The Report concludes that the proposed development (within Flood Zone 1) is at a low risk of flooding. With regard to SuDS, infiltration based techniques are not considered appropriate due to site contamination issues.
- 6.5.13. Although the conclusions of the submitted Report are acknowledged, the points raised by the SuDS Officer are also noted. It is considered that a condition could be reasonably imposed to secure a drainage strategy and management plan to address the concerns of the SuDS Officer.

Site Waste Management

Construction Waste

- 6.5.14. Policy 5.16 of the London Plan has stated goals of working towards managing the equivalent of 100% of London’s waste within London by 2031 (by 2026 as stated in FALP), creating benefits from waste processing and zero biodegradable or recyclable waste to landfill by 2031. This will be achieved in part through exceeding recycling and reuse levels in construction, excavation and demolition (“CE&D”) waste of 95% by 2020.
- 6.5.15. In order to achieve the above, London Plan policy 5.18 confirms that through the Local Plan, developers should be required to produce site waste management plans to arrange for the efficient handling of CE&D. Core Policy 22 of the Core Strategy states that the Council will encourage on-site reuse and recycling of CE&D waste.
- 6.5.16. Details of a construction waste management plan have not been submitted with the application. This detail can be secured through an appropriately worded condition.

Operational Waste

6.5.17. To understand the potential waste generation of the facility whilst in operation, a Waste Management Plan (“WMP”) should be provided detailing the waste minimisation proposals and the proposed strategy for dealing with waste generated from the use of the site. Whilst one has not been provided, a WMP can reasonably be secured by condition.

Contamination

6.5.18. Having regard to the existing use of the site as a nursery, and the former use as an orchard, consideration must be given to land contamination (London Plan policy 5.22 and DMD66). To enable an understanding of any potential contaminants and the likely impact on receptors from these former uses, a *Land Appraisal and Contamination Report* has been submitted.

6.5.19. The Report identifies that the site was previously used as an industrial landfill operated by Johnson Matthey between 1913 and 1958. As a result of this, near-surface contamination does exist, posing either a physical or chemical risk. A remediation strategy has been proposed which includes a 600mm capping layer, thickened to 1m because of the need to provide drains to a depth of 400mm to ensure adequate drainage of the playing surface.

6.5.20. Although capping could potentially result in landfill gas migration, it is considered that the risk from this is low due to the age and nature of the waste. However, it is proposed to install perimeter monitoring points during the remediation phase to record existing levels of methane and carbon dioxide.

6.5.21. A quantitative risk assessment will need to be provided and an assessment on the risk to controlled waters. The development should not commence until a scheme to deal with the contamination of the site including an investigation and assessment of the extent of contamination and the measure to be taken to avoid risk to health and the environment.

6.5.22. The importation of waste is governed by the Environment Agency’s Permitting Regime.

6.6. Employment and Training

6.6.1. Core Policy 16 of the Core Strategy confirms the commitment of the Council to promote economic prosperity and sustainability in the Borough through a robust strategy to improve the skills of Enfield’s population. One initiative is, through the collaboration with the Boroughs of Haringey, Broxbourne, Epping and Waltham Forest, to promote skills training for local people.

6.6.2. The Strategy will need to set out how the development will engage with local contractors / subcontractors, the number of trainees to be employed on site and the number of weeks training will be provided. Details of a Local Employment Strategy could be secured by condition.

6.7. Community Infrastructure Levy

6.7.1. The development is not CIL liable as it is for a leisure / community use.

6.8. Section 106

6.8.1. Having regard to the contents of the content above, it is recommended that should planning permission be granted, the following obligations should be sought:

- Securing the local sourcing of labour
- Securing the local supply of goods and materials
- Securing on-site skills training
- Entering into a s278 Agreement for associated highways works

6.9. Other Matters Raised

6.9.1. Boundary issues are a civil matter

7. Conclusions

7.1. The development proposal is considered to make a more effective use of the site to provide additional sport, recreational and community facilities. Moreover, there would be no additional impact from the provision of the clubrooms than the existing despite its greater size.

7.2. Taking all material planning considerations into account it is considered that the development should be approved for the following reasons:

1. The proposed development, having regard to its size, siting and design and by virtue of conditions imposed has appropriate regard to its surroundings, the character and amenities of the local area and those of adjoining occupiers in terms of loss of light, privacy, outlook, noise and disturbance, having regard to Policies 7.1, 7.4 & 7.6 7.15 of The London Plan, Core Policy 30, Policies DMD8, DMD10, DMD11, DMD68 of the Development Management Document.
2. The development makes appropriate provision for access and servicing and should not lead to conditions detrimental to highway safety on having regard to Policy 6.3 of The London Plan, DMD47 of the Development Management Document.
3. The proposed development, by virtue of the measures proposed and conditions imposed, should achieve an acceptable level of sustainable design and construction having regard to Policies 5.1, 5.2, 5.3, 5.6, 5.7, 5.8 & 5.9 of the London Plan, Core Policies 20, 21, 22, & 26 of the Core Strategy, Policies DMD49, DMD51, DMD53, DMD55, DMD56, DMD58, DMD59, DMD60, DMD61, DMD69, DMD78, DMD79, DMD81 of the Development Management Document.

8. Recommendation

8.1. That planning permission is GRANTED subject to conditions:

1. Approved Plans – as Amended
Unless required by any other condition attached to this permission, the development shall be undertaken in accordance with the approved plans, including plans that may have been revised, as set out in the attached schedule which forms part of this notice.

Reason: For the avoidance of doubt and in the interest of proper planning.

2. Time Limited Permission

The development to which this permission relates must be begun not later than the expiration of three years beginning with the date of the decision notice.

Reason: To comply with the provisions of S.51 of the Planning & Compulsory Purchase Act 2004

3. Details of Materials

Above ground construction works shall not commence until details of the external finishing materials to be used have been submitted to and approved in writing by the Local Planning Authority. The development shall be constructed in accordance with the approved details.

Reason: To ensure a satisfactory external appearance.

4. Levels

Unless otherwise approved in writing, the development hereby approved shall only be undertaken in accordance with the detailed levels as provided on Drawing No.101.

Reason: To ensure that levels have appropriate regard to the level of surrounding development, gradients and surface water drainage.

5. Details of Hard Surfacing

Above ground construction works shall not commence until details of the surfacing materials to be used within the development including footpaths, access roads and parking areas and road markings have been submitted to and approved in writing by the Local Planning Authority. The surfacing shall be carried out in accordance with the approved detail before the development is occupied or use commences.

Reason: To ensure that the development does not prejudice highway safety and a satisfactory appearance.

6. Highways Related Audits

The development hereby approved shall not commence until such time as the following audits have been submitted to and approved in writing by the Local Planning Authority:

- a. Stage 2 Road Safety Audit;
- b. PERS Audit between A1055 and A1010; and
- c. CLoS Audit between A1055 and A1010

The works shall be carried out in accordance with the approved detail before the development is occupied or use commences.

Reason: In the interest of highway safety.

7. Construction Methodology

Development shall not commence until a construction methodology has been submitted to and approved in writing by the Local Planning Authority. The construction methodology shall contain:

- a. a photographic condition survey of the roads, footways and verges immediately adjacent to the site;

- b. details of construction access, including any temporary heavy duty access, and associated traffic management to the site;
- c. details of the phasing of construction works;
- d. arrangements for the loading, unloading and turning of delivery, construction and service vehicles clear of the highway;
- e. arrangements for the parking of contractors vehicles;
- f. arrangements for wheel cleaning;
- g. arrangements for the storage of materials;
- h. hours of work;
- i. number and type of vehicle movements;
- j. Coordination with local schools to avoid peak school drop-off and pick up times;
- k. A construction management plan written in accordance with the 'London Best Practice Guidance: The control of dust and emission from construction and demolition';
- l. size and siting of any ancillary buildings.

The development shall be carried out in accordance with the approved construction methodology unless otherwise agreed in writing by the Local Planning Authority.

Reason: To ensure the implementation of the development does not lead to damage to the existing highway and to minimise disruption to neighbouring properties and the environment.

8. Parking / Turning Facilities

Notwithstanding the submitted proposed site layout plan, the development shall not commence until a revised plan has been submitted to the Local Planning Authority and approved in writing, detailing parking and turning facilities to be provided in accordance with the standards adopted by the Local Planning Authority, inclusive of suitable parking for minibuses and coaches. The parking facilities shall be constructed in accordance with the approved details before the development is occupied and shall be maintained for this purpose.

Reason: To ensure that the development complies with adopted standards and does not prejudice conditions of safety or traffic flow on adjoining highways.

9. Loading / Unloading / Turning Facilities

The development shall not commence until details showing facilities for the loading, unloading and turning of vehicles clear of the highway have been submitted to and approved in writing by the Local Planning Authority. The facilities shall be constructed in accordance with the approved details before the development is occupied and shall be kept free from obstruction and maintained for this purpose.

10. Reason: To ensure that the development complies adopted Policy and does not prejudice conditions of safety or traffic flow on adjoining highways.

11. Details of Access and Junction

The development shall not commence until details of the construction of any access roads and junctions and any other highway alterations associated with the development (inclusive of pedestrian paths linking the car park to the clubrooms) have been submitted to and approved in writing by the Local Planning Authority. These works shall be carried out in accordance with the approved details before development is occupied or the use commences.

Reason: To ensure that the development complies with adopted Policy and does not prejudice conditions of safety or traffic flow on adjoining highways.

12. Swept Path Analysis

The development shall not commence until a swept path analysis demonstrating acceptable access and manoeuvring within the site for emergency vehicles and coaches has been provided to the Local Planning Authority and approved in writing.

Reason: To ensure that the development complies adopted Policy and does not prejudice conditions of safety or traffic flow on adjoining highways.

13. Electric Charging Points

Prior to development commencing, details for electric vehicle charging points (no less than 10% active and 10% passive of the total parking provision proposed) shall be provided to the Local Planning Authority for approval in writing. All electric charging points shall be installed in accordance with the approved details prior to first use of the facilities approved and permanently retained and maintained thereafter.

Reason: To ensure that the development complies with the sustainable development policy requirements of the London Plan.

14. Cycle Parking

The details of the secure covered cycle parking facilities shall be submitted for approval in writing by the Local Planning Authority. The facilities shall be provided in accordance with the approved details prior to the first use of the development authorised by this permission.

Reason: To encourage sustainable means of transport and to ensure that the design, size, siting and materials of the cycle store are appropriate to the architectural and historic interest of the Listed Building.

15. Car Park Management Plan

Prior to the commencement of the development hereby permitted, details showing facilities and strategy to manage delivery/taxis/car and coach arrivals and departures clear of the highway shall be submitted to and approved in writing by the Local Planning Authority. The strategy shall be adopted and facilities shall be constructed in accordance with the approved details before the development is occupied and shall be kept free from obstruction and maintained for the duration of the use unless the prior written approval of the Local Planning Authority is obtained to any variation. The Strategy should include:

1. Car park plan with clearly marked out bays;
2. Contact details (telephone/ email ,name) of a person/company responsible for managing the car park;
3. Proposed parking controls proposed to keep clear the turning areas for delivery/service vehicles;
4. Management of the risk of overspill parking;
5. How will pedestrian access routes be kept clear?
6. How will visitors be accommodated;
7. What enforcement regime is proposed;
8. How often will the plan be reviewed and by whom (provide contact details);
9. Maintenance regime for apparatus/furniture –lighting etc;

10. Details of parking charges/fees;
11. Number of 'accessible'/disabled bays, disabled bays distribution and monitoring of take up and how it connects with the Travel Plan; and
12. Proposed type car parking spaces allocation across site (first come first served basis, etc.).

Reason: To ensure that the proposals do not prejudice conditions of safety or traffic flow on adjoining highways and does not adversely impact on operation and parking provision of the nearby uses and existing residents.

16. Deliveries and Collections

Deliveries and collections to and from the premises shall only take place between the hours of 0700hours and 2000hours Monday to Saturday only and not at all on Sundays and Bank Holidays, unless otherwise agreed in writing by the Local Planning Authority.

Reason: To safeguard the amenities of the occupiers of nearby residential properties.

17. Details of Refuse Storage

Within three months of commencement of the development approved, details of refuse storage facilities (design, size, siting) including facilities for the recycling of waste to be provided within the development, in accordance with the London Borough of Enfield – Waste and Recycling Planning Storage Guidance ENV 08/162, have been submitted to and approved in writing by the Local Planning Authority. The refuse storage facilities shall be provided in accordance with the approved details before the development is occupied or use commences.

Reason: In the interest of amenity and the recycling of waste materials in support of the Boroughs waste reduction targets.

18. Details of Fume Extraction

Prior to first use of the kitchen facilities, details of the specification and appearance of any fume extraction and/or ventilation plant required in connection with the use shall be submitted to the Local Planning Authority for approval in writing. The plant shall be installed in accordance with the approved details and permanently maintained.

Reason: To ensure a satisfactory appearance and safeguard amenity.

19. Contamination - Remediation Strategy

No development approved by this planning permission shall take place until a remediation strategy that includes the following components to deal with the risks associated with contamination of the site shall each be submitted to and approved, in writing, by the local planning authority:

1. A preliminary risk assessment which has identified:
 - a. all previous uses
 - b. potential contaminants associated with those uses
 - c. a conceptual model of the site indicating sources, pathways and receptors
 - d. potentially unacceptable risks arising from contamination at the site.
2. A site investigation scheme, based on (1) to provide information for a detailed assessment of the risk to all receptors that may be affected, including those off site.

3. The results of the site investigation and the detailed risk assessment referred to in (2) and, based on these, an options appraisal and remediation strategy giving full details of the remediation measures required and how they are to be undertaken.
4. A verification plan providing details of the data that will be collected in order to demonstrate that the works set out in the remediation strategy in (3) are complete and identifying any requirements for longer-term monitoring of pollutant linkages, maintenance and arrangements for contingency action.

Any changes to these components require the express written consent of the local planning authority. The scheme shall be implemented as approved.

Reason: To prevent both new and existing development from contributing to or being put at unacceptable risk from, or being adversely affected by unacceptable levels of water pollution.

20. Contamination - Verification Report

No occupation of any part of the permitted shall take place until a verification report demonstrating completion of works set out in the approved remediation strategy and the effectiveness of the remediation shall be submitted to and approved, in writing, by the local planning authority. The report shall include results of sampling and monitoring carried out in accordance with the approved verification plan to demonstrate that the site remediation criteria have been met. It shall also include any plan (a "long-term monitoring and maintenance plan") for longer-term monitoring of pollutant linkages, maintenance and arrangements for contingency action, as identified in the verification plan. The long-term monitoring and maintenance plan shall be implemented as approved.

Reason: To prevent both new and existing development from contributing to or being put at unacceptable risk from, or being adversely affected by unacceptable levels of water pollution.

21. Contamination – Additional contamination

If, during development, contamination not previously identified is found to be present at the site then no further development (unless otherwise agreed in writing with the local planning authority) shall be carried out until the developer has submitted a remediation strategy to the local planning authority detailing how this unsuspected contamination shall be dealt with and obtained written approval from the local planning authority. The remediation strategy shall be implemented as approved.

Reason: To prevent both new and existing development from contributing to or being put at unacceptable risk from, or being adversely affected by unacceptable levels of water pollution.

22. Infiltration

No infiltration of surface water drainage into the ground is permitted other than with the express written consent of the local planning authority, which may be given for those parts of the site where it has been demonstrated that there is no resultant unacceptable risk to controlled waters. The development shall be carried out in accordance with the approval details.

Reason: To prevent both new and existing development from contributing to or being put at unacceptable risk from, or being adversely affected by unacceptable levels of water pollution.

23. Sub-surface works

Piling or any other foundation designs / investigation boreholes / tunnel shafts / ground source heating and cooling systems using penetrative methods shall not be permitted other than with the express written consent of the local planning authority, which may be given for those parts of the site where it has been demonstrated that there is no resultant unacceptable risk to groundwater. The development shall be carried out in accordance with the approved details.

Reason: To prevent both new and existing development from contributing to or being put at unacceptable risk from, or being adversely affected by unacceptable levels of water pollution. Piling or any other foundation designs / investigation boreholes / tunnel shafts / ground source heating and cooling systems using penetrative methods can result in risks to potable supplies from, for example, pollution / turbidity, risk of mobilising contamination, drilling through different aquifers and creating preferential pathways.

24. Restriction of Open Storage

No plant, machinery, goods, products or waste material shall be deposited or stored on any open part of the site unless otherwise agreed in writing by the Local Planning Authority.

Reason: In the interest of amenity and the appearance of the site.

25. Energy

Notwithstanding any submitted document, development shall not commence until an Energy Statement, detailing the measures to be implemented in order to meet with or exceed a 35% improvement over Part L of the 2013 Building Regulations, inclusive of the use of renewable energy technologies, has been submitted to the Local Planning Authority and approved in writing. Where renewable energy technologies are to be utilised, for each of the renewable energy technologies that are considered feasible, the detail shall include:

- (a) The resulting scheme, together with any machinery/apparatus location, specification and operational details;
- (b) A management plan and maintenance strategy/schedule for the operation of the technologies;
- (c) A servicing plan including times, location, frequency, methodology.

The development shall be carried out strictly in accordance with the details so approved and shall be maintained as such thereafter.

Reason: In the interest of sustainable development and to ensure that the Local Planning Authority may be satisfied that CO2 emission reduction targets by renewable energy are met in accordance with adopted policy.

26. Energy Performance Certificate

Following practical completion of works a final Energy Performance Certificate shall be submitted to the Local Planning Authority for approval in writing. Where applicable, a Display Energy Certificate shall be submitted within 18 months following first occupation.

Reason: In the interest of sustainable development and to ensure that the Local Planning Authority is satisfied that CO2 emission reduction targets are met in accordance with adopted policy.

27. Biodiverse / Green Roof

The development shall not commence until details have been provided to the Local Planning Authority for approval in writing demonstrating the feasibility or otherwise of providing a biodiverse green / brown roof. The submitted detail shall include: design, substrate (extensive substrate base with a minimum depth 80-150mm), vegetation mix and density, and a cross-section of the proposed roof.

The biodiverse roof shall not be used for any recreational purpose and access shall only be for the purposes of the maintenance and repair or means of emergency escape.

The biodiverse roof shall be implemented in accordance with the approved details prior to first occupation and maintained as such thereafter. Photographic evidence of installation is to be submitted and approved in writing by the Local Planning Authority.

Reason: To assist in flood attenuation and to ensure the development provides the maximum possible provision towards the creation of habitats and valuable areas for biodiversity in accordance with adopted Policy.

28. Living Walls

Details of the feasibility for providing "living walls" to the approved clubhouse shall be submitted to the Local Planning Authority for approval in writing prior to first use commencing. The submitted details shall include:

- (a) Locations for planting of "living walls";
- (b) Type and density of native wildlife friendly plantings;

Should the Local Planning Authority consider that the provision of living walls is feasible, plantings shall be provided within the first planting season following practical completion of the development. Any planting which dies, becomes severely damaged or diseased within five years of completion of the development shall be replaced with new planting in accordance with the approved details or an alternative approved in writing by the Local Planning Authority.

Reason: To enhance the ecological value of the site and to ensure the development provides the maximum possible provision towards the creation of habitats and valuable areas for biodiversity in accordance with adopted policy, and to ensure highway safety.

29. Landscaping

Notwithstanding any submitted plan, a landscaping plan shall be submitted to the Local planning Authority for approval in writing prior to any works commencing. The Landscaping plan shall include:

- a. Planting plans to include enhanced planting around the entire perimeter of the site;
- b. Written specifications (including cultivation, maintenance and other operations associated with plant and grass establishment);
- c. Schedules of plants and trees, to include native and wildlife friendly species and large canopy trees in appropriate locations (noting species, planting sizes and proposed numbers / densities);
- d. Implementation timetables;
- e. Wildlife friendly plants and trees of local or national provenance;
- f. Specifications for any boundary treatment demonstrating how hedgehogs and other wildlife will be able to continue to travel across the site.

Plantings shall be provided no later than the first planting season following practical completion of the development. Any planting which dies, becomes severely damaged or diseased within five years of completion of the development shall be replaced with new planting in accordance with the approved details or an alternative approved in writing by the Local Planning Authority.

Reason: To enhance the ecological value of the site, to ensure the development provides the maximum possible provision towards the creation of habitats and valuable areas for biodiversity in accordance with adopted policy, to ensure highway safety and to safeguard the residential amenity of neighbouring occupiers.

30. Tree Protection

No works or development shall take place until an Arboricultural Method Statement (BS5837:2012) has been submitted to the Local Planning Authority and approved in writing. The Method Statement shall ensure, where necessary, that any retained tree is sufficiently protected from increased ground levels and no works are undertaken within the root protection area of the retained tree.

Reason - To ensure that the retained trees, shrubs and hedgerows on the site or in adjacent sites are not adversely affected by any aspect of the development, and to screen, preserve and enhance the development and ensure adequate landscape treatment in the interest of amenity, having regard to adopted Policy.

31. Vegetation Clearance

All areas of trees, hedges, scrub or similar vegetation where birds may nest, which are to be removed as part of the development, are to be cleared outside the bird-nesting season (March - August inclusive) or if clearance during the bird-nesting season cannot reasonably be avoided, a suitably qualified ecologist will check the areas to be removed immediately prior to clearance and advise whether nesting birds are present. If active nests are recorded, no vegetation clearance or other works that may disturb active nests shall proceed until all young have fledged the nest.

Reason: To ensure that nesting birds are not adversely affected upon by the proposed development.

32. Biodiversity Enhancements

The enhancement proposals as set out within section 4.2 of the Ecological Appraisal shall be implemented in accordance with details (design, size, number, location, as appropriate) to be provided to the Local Planning Authority for approval in writing. The submitted detail shall also include a Nature Conservation Management Plan for all new and retained habitat, to include plant replacement as necessary, establishment maintenance, followed by a strategy of low key, appropriate, conservation management and monitoring.

Reason: To enhance the site post development in line with Core Policy 36.

33. Details of Spectator Stands

No spectator stand shall be erected until their details (inclusive of design and materials) have been submitted to the Local Planning Authority and approved in writing. The spectator stands shall be erected in accordance with the approved detail and permanently maintained.

Reason: To ensure an acceptable appearance and in the interest of safeguarding the privacy of neighbouring residential occupiers.

34. Drainage

Development shall not commence until a revised drainage scheme has been submitted to the Local Planning Authority and approved in writing. The drainage scheme shall dispose of surface water by means of a sustainable drainage (SUDS) scheme, in accordance with the principles of sustainable drainage systems set out in national planning policy guidance and statements, and the results of that assessment have been provided to the Local Planning Authority. The assessment shall take the following into account:

- a. Source control measures (e.g. green roofs, rain gardens, permeable paving);
- b. The use of lined swales, ponds, basins;
- c. Details of levels, cross sections and specifications of the drainage features;
- d. A design which allows for flows that exceed the design capacity to be stored on site or conveyed off-site with minimum impact
- e. A clear rationale for the proposed SUDS measures;
- f. A Management Plan outlining the specific actions required to ensure long-term maintenance;
- g. the design storm period and intensity for a 1 in 100 year storm event with the allowance for climate change; and
- h. measures to prevent pollution of the receiving groundwater and/or surface waters

The approved drainage measures shall be implemented and permanently maintained.

Reason: To ensure that the proposal would not result in an unacceptable risk of flooding from surface water run-off or create an unacceptable risk of flooding elsewhere, and to reduce the risk of pollution.

35. Drainage Verification

Prior to occupation of the development, a Verification Report demonstrating that the approved drainage / SuDS measures have been fully implemented shall be submitted to the Local Planning Authority for approval in writing. This report must include:

- a. As built drawings of the sustainable drainage systems
- b. Level surveys of completed works
- c. Photographs of the completed sustainable drainage systems
- d. Any relevant certificates from manufacturers/ suppliers of any drainage features
- e. A confirmation statement of the above signed by a chartered engineer

Reason : To ensure the sustainable management of water, minimise flood risk, minimise discharge of surface water outside of the curtilage of the property and ensure that the drainage system will remain functional throughout the lifetime of the development in accordance with Policy CP28 of the Core Strategy and Policies 5.12 & 5.13 of the London Plan and the NPPF

36. Noise Attenuation

The development shall not commence until details of the design, size, siting and minimum density of an acoustic bund / barrier to limit sound generated from the use of the site being audible beyond the site boundary has been submitted to the Local Planning Authority and approved in writing.

Reason: To minimise any additional impact on neighbour amenity from noise generated by the approved development and to

37. Ongoing Noise Mitigation

The use of the development authorised by this permission shall not begin until a written scheme of ongoing noise monitoring and mitigation has been submitted to and approved in writing by the Local Planning Authority.

Reason: To protect the existing amenity of nearby residential occupiers and to ensure that any mitigation measures are effective.

38. Lighting – Car Park

No works shall commence on the provision and installation of external lighting along the access road and car park until their details have been submitted to the Local Planning Authority and approved in writing. The details shall include:

- a. Design;
- b. Siting;
- c. Lux levels;
- d. Measures to limit the impact of light spillage onto light sensitive premises.

The approved lighting shall be provided before the development is occupied or first use commences.

Reason: In the interest of protecting the amenity of the residential occupiers and the minimal disruption to wildlife friendly habitats whilst being cognizant of the need to provide suitable levels of lighting.

39. Lighting – Floodlights

No floodlight shall be erected until the following details have been submitted to the Local Planning Authority and approved in writing:

1. Illuminance plots to demonstrate impact on all adjoining residential properties;
2. Measures to secure directional control
3. Number of lights per column;
4. Luminance levels; and
5. Mitigation measures to reduce light spillage

The floodlights shall only be erected in accordance with the approved detail.

Reason: To safeguard the amenity of neighbouring residential occupiers and

40. Hours of Use – Sports Pitches

Unless otherwise agreed in advance and in writing by the Local Planning Authority, the artificial grass pitch, grass sports pitch and multi-use games areas and its associated sports lighting shall not be used outside the hours of 0800hours and 2200hours Monday to Saturday and 0800hours and 2000hours on Sundays and Bank Holidays.

Reason: To balance illuminating the [named playing field/sports facility] for maximum use with the interest of amenity and sustainability and to accord with Development Plan Policy

41. Hours of Use – Clubroom facilities

The clubroom facilities hereby approved shall only be open between the hours of 0800hours and 2300hours Monday to Saturday and 0800hours and 2100hours

on Sundays and Bank Holidays, unless otherwise approved in writing by the Local Planning Authority.

Reason: To safeguard the amenities of the occupiers of nearby residential properties.

42. Artificial Pitch Construction

The artificial grass pitch and Multi-Use Games Areas hereby permitted shall not be constructed other than substantially in accordance with Sport England/National Governing Body Technical Design Guidance Notes; Artificial Surfaces for Outdoor Sport or FA Guide to Football Turf Pitch Designs and Layouts

Reason: To ensure the development is fit for purpose and sustainable and to accord with Development Plan Policy

43. Artificial Pitch

Before the artificial grass pitch is brought into use, a Management and Maintenance Scheme for the facility including management responsibilities, a maintenance schedule and a mechanism for review shall be submitted to and approved in writing by the Local Planning Authority. This should include measures to ensure that the surface is replaced at the end of its usual lifespan. The measures set out in the approved scheme shall be complied with in full, with effect from commencement of use of the artificial grass pitch.

Reason: To ensure that a new facility is capable of being managed and maintained to deliver a facility which is fit for purpose, sustainable and to ensure sufficient benefit of the development to sport and to accord with Development Plan Policy

44. Construction Waste Management Plan

The development shall not commence until a Construction Waste Management Plan has been submitted to the Local Planning Authority for approval in writing. The plan should include as a minimum:

- a. Target benchmarks for resource efficiency set in accordance with best practice
- b. Procedures and commitments to minimize non-hazardous construction waste at design stage. Specify waste minimisation actions relating to at least 3 waste groups and support them by appropriate monitoring of waste.
- c. Procedures for minimising hazardous waste
- d. Monitoring, measuring and reporting of hazardous and non-hazardous site waste production according to the defined waste groups (according to the waste streams generated by the scope of the works)
- e. Procedures and commitments to sort and divert waste from landfill in accordance with the waste hierarchy (reduce; reuse; recycle; recover) according to the defined waste groups

In addition no less than 85% by weight or by volume of non-hazardous construction, excavation and demolition waste generated by the development has been diverted from landfill

Reason: To maximise the amount of waste diverted from landfill consistent with the waste hierarchy and strategic targets set by Policies 5.17, 5.18, 5.19, 5.20 of the London Plan.

AMENDMENTS:



NOTE:
 THE INFORMATION OF THIS DRAWING
 IS FOR INFORMATION ONLY. UNLESS OTHERWISE
 STATED, THE INFORMATION IS NOT TO BE
 USED FOR ANY OTHER PURPOSES.
 ALL EXISTING AND PROPOSED DIMENSIONS AND LEVELS MUST BE
 VERIFIED ON-SITE BEFORE COMMENCING WORK.

PROPOSED DEVELOPMENT AT
 BRIMSDOWN SPORTS GROUND
 BRIFIELD

151010631 FULL

MCLAREN ASSOCIATES
 ARCHITECTS AND URBAN CONSULTANTS

BRIMSDOWN
 SITE LOCATION

PROPOSED DEVELOPMENT AT
 BRIMSDOWN SPORTS GROUND
 BRIFIELD

DATE	11/2010 (AJ)
DATE	APRIL 2011
DATE	2011
DATE	2011
DATE	2011

868 61

BRIMSDOWN SPORTS GROUND, ENFIELD



AMENDMENTS:

NOTE:
 THIS PLAN IS A PRELIMINARY DESIGN AND IS NOT TO BE USED FOR CONSTRUCTION.
 IT IS THE RESPONSIBILITY OF THE CLIENT TO OBTAIN ALL NECESSARY PERMITS AND APPROVALS.
 NO GUARANTEE OF ACCURACY IS GIVEN.
 ALL EXISTING AND PROPOSED DIMENSIONS AND LEVELS MUST BE VERIFIED ON SITE BEFORE COMMENCING WORK.

DATE: 15/10/2015
 DRAWN BY: J. MCLAREN
 CHECKED BY: S. MCLAREN
 PROJECT: BRIMS DOWNS SPORTS GROUND
 SITE: ENFIELD

15/10/2015 | FUL

MCLAREN ASSOCIATES
 ARCHITECTURAL AND DESIGN CONSULTANTS

SCALE: 1:500

SITE AMENDMENT
 AS EXISTING

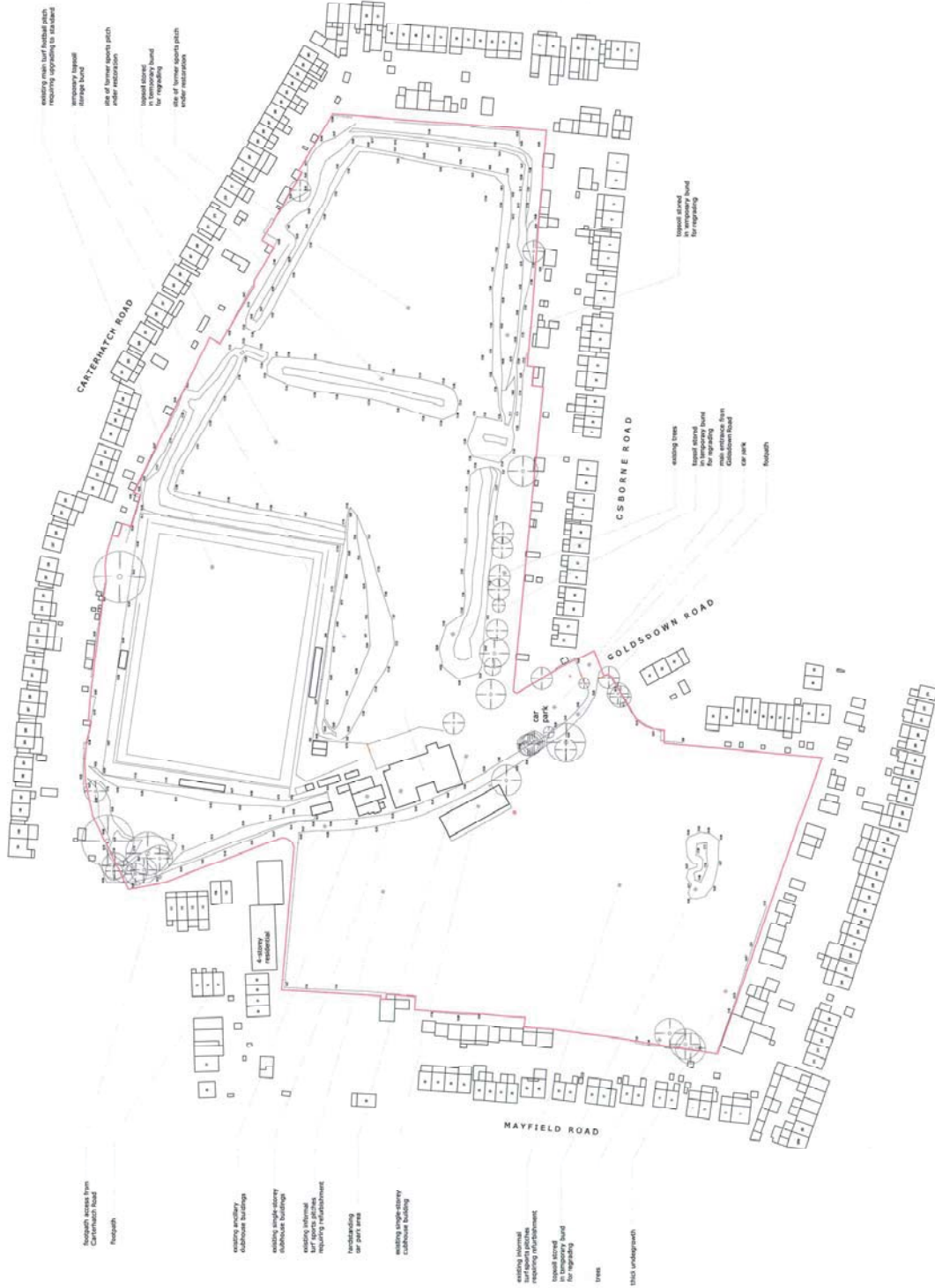
BRIMS DOWNS SPORTS GROUND
 7 BRIMS DOWNS ROAD, ENFIELD, MIDDLESEX, ENGLAND

DATE: 15/10/2015

BY: J. MCLAREN

PROJECT NO: 868

103



BRIMS DOWNS SPORTS GROUND, ENFIELD



MATERIALS
 WALLS: DARK STAINED HORIZONTAL TIMBER BOARDING
 ROOF: DARK GREY AVON SLATE OR CEILING SHINGLES
 FLOORING: POLISHED CONCRETE
 PAINTWORK: DARK BROWN POWDER-COATED ALUMINIUM FINISHING TRIMMER FRAMES
 BATHROOMS: DARK BROWN POLISHED HARDWOOD FLOORING
 FLOORING: POLISHED CONCRETE

NOTES
 THIS DRAWING IS PREPARED FROM A LARGER UNCHECKED DOCUMENT
 NO GUARANTEE OF ACCURACY IS GIVEN
 UNTIL A DIMENSIONAL SURVEY IS COMPLETED
 ALL EXISTING AND PROPOSED DIMENSIONS AND LEVELS MUST BE VERIFIED
 IN THE FIELD BEFORE COMMENCING WORK

INDICATED DEVELOPMENT AT
 BRIMS DOWNS SPORTS GROUND

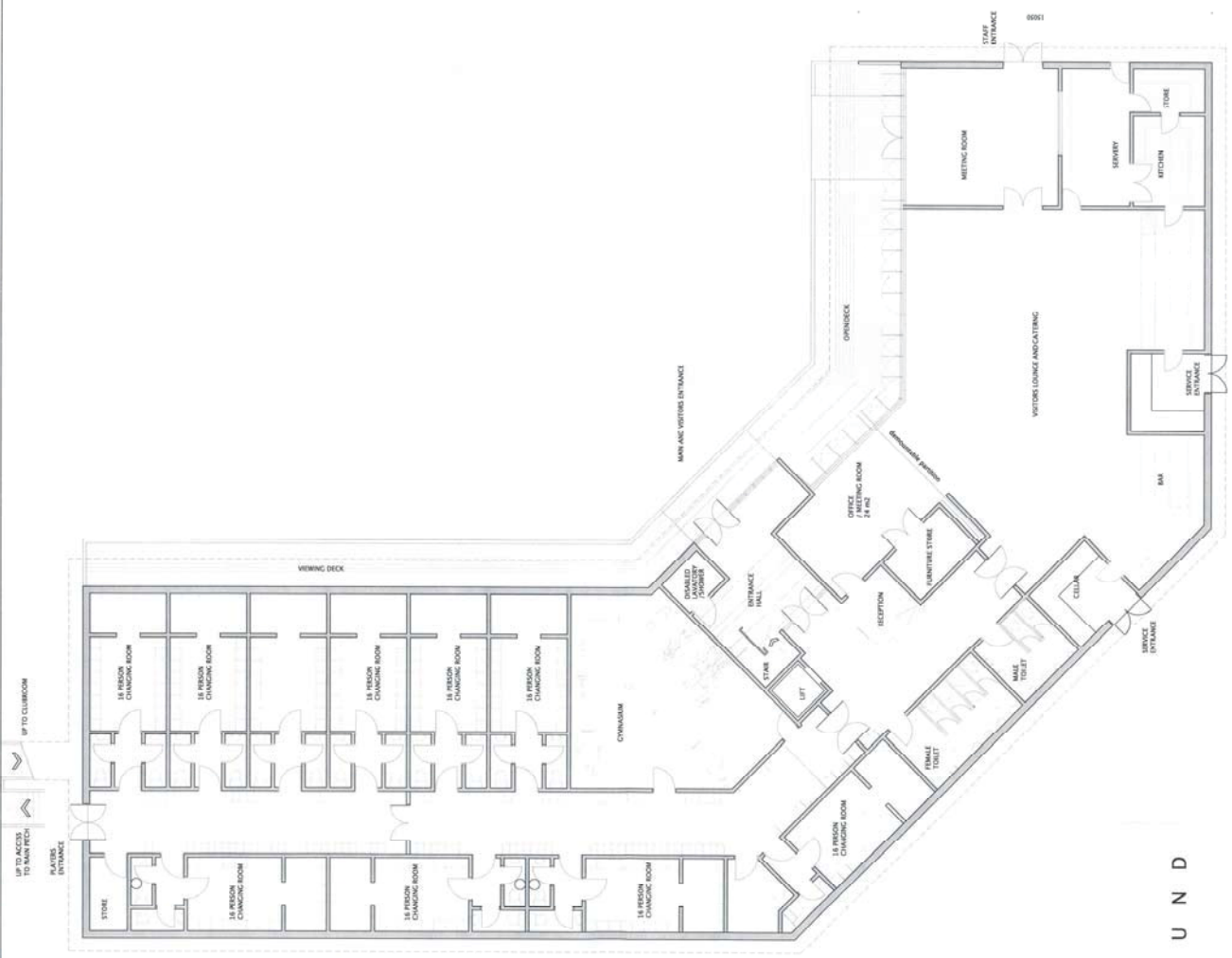
15101631FUL

MCLAREN ASSOCIATES
 ARCHITECTURAL AND DESIGN CONSULTANTS
 15101631 PROJECT | BRIMS DOWNS SPORTS GROUND | BRIMS DOWNS

INDICATED DEVELOPMENT AT
 BRIMS DOWNS SPORTS GROUND
 BRIMS DOWNS SPORTS GROUND
 BRIMS DOWNS SPORTS GROUND

FOR PLANNING

DATE	1.10.16 (A1)	DATE	JANUARY 2016
SCALE	1:100	CLIENT	
SP		PROJECT NUMBER	868
			105



BRIMS DOWNS
 SPORTS GROUND
 ENFIELD

GROUND FLOOR PLAN



MATERIALS:
 WALLS: DARK STAINED HORIZONTAL TIMBER BOARDING
 DOOR: UNPAINTED WHITE PINE
 FLOOR: POLISHED CONCRETE
 CEILING: DARK STAINED HORIZONTAL TIMBER BOARDING
 LIGHTING: RECESSED LED DOWNLIGHTERS
 FURNITURE: CUSTOM DESIGNED
 FINISHES: AS PER ARCHITECTURAL SCHEDULE

NOTE:
 THIS DRAWING IS PREPARED FROM LATEST UNCHECKED DOCUMENTS
 NO WARRANTY OF ACCURACY IS GIVEN
 ALL DIMENSIONS AND LEVELS MUST BE VERIFIED
 ON-SITE BEFORE COMMENCING WORK

PROPOSED DEVELOPMENT AT
 BRIMSDOWN SPORTS GROUND
 ENFIELD

1510531FUL

MCLAREN ASSOCIATES
 ARCHITECTURAL AND DESIGN CONSULTANTS
 1510531 PROJECT - BRIMSDOWN SPORTS GROUND
 UPPER FLOOR PLAN

PROPOSED REPLACEMENT
 CLUBHOUSE AND FACILITIES BUILDING
 UPPER FLOOR PLAN

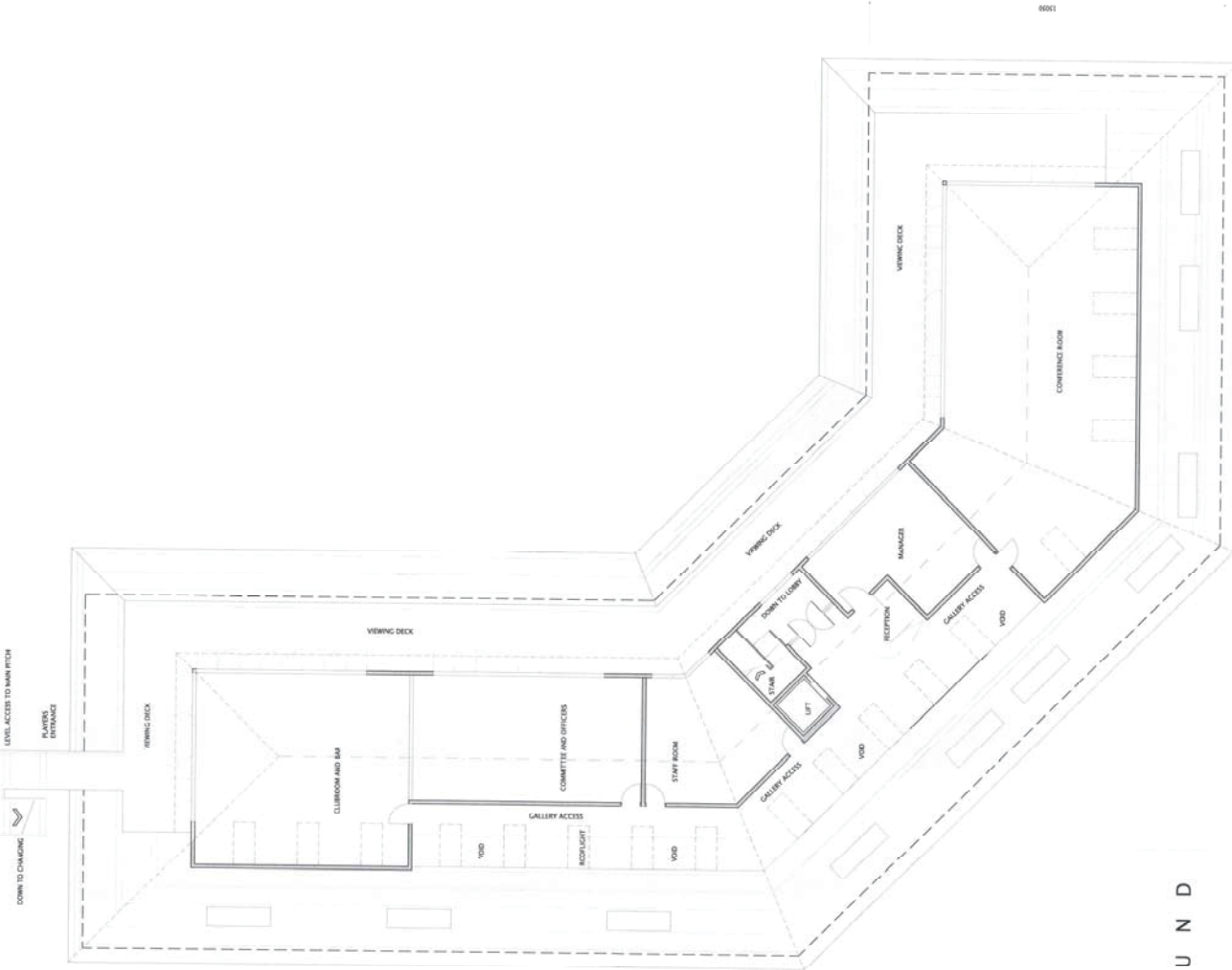
FOR PLANNING

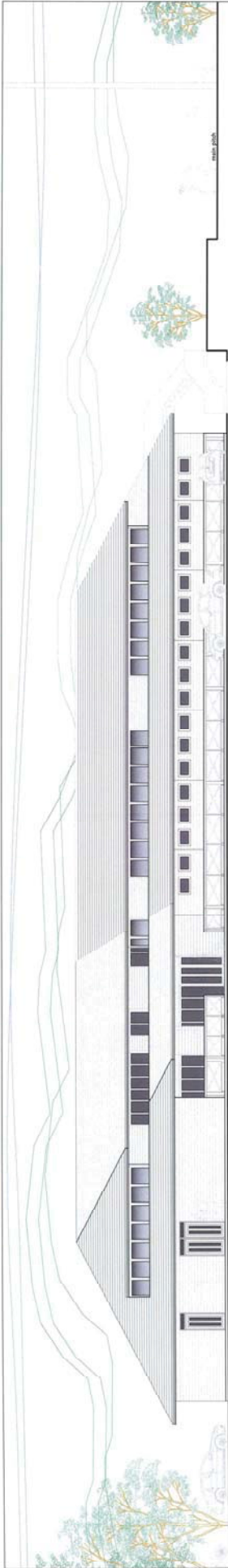
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SCALE: 1:500	SCALE: 1:500
NO: 868	NO: 106

BRIMSDOWN SPORTS GROUND
 - E 111 - 1000
 DEVELOPMENT - 2014/15

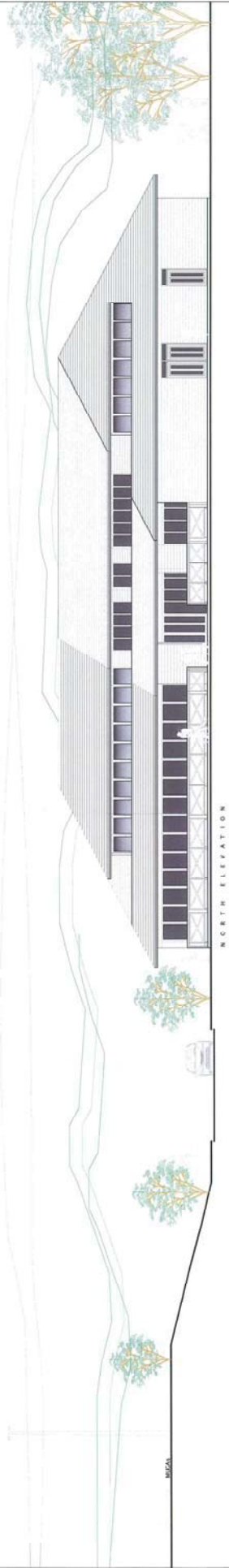
UPPER FLOOR PLAN

BRIMSDOWN
 SPORTS GROUND
 ENFIELD

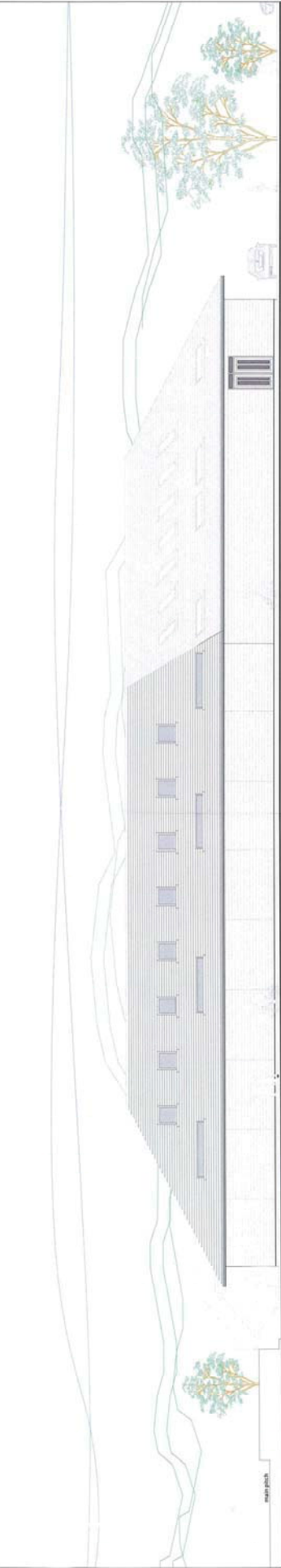




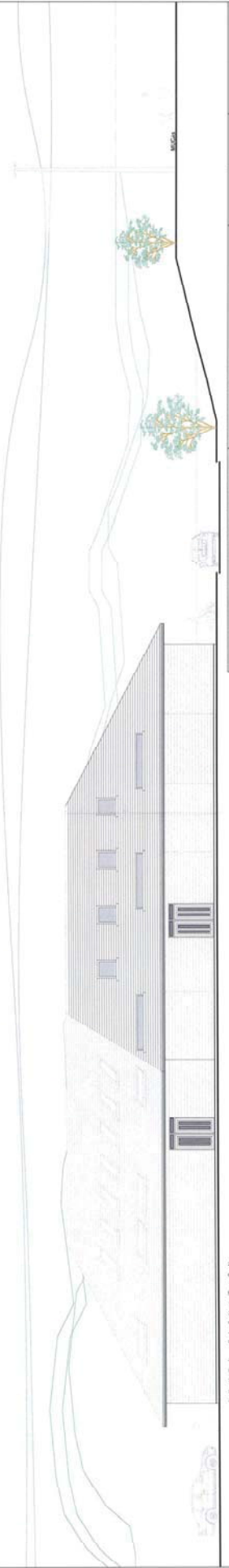
EAST ELEVATION



NORTH ELEVATION



WEST ELEVATION



SOUTH ELEVATION



BRIMS DOWNS SPORTS GROUND, ENFIELD

DATE	15/10/2016
PROJECT	PROPOSED DEVELOPMENT AT BRIMS DOWNS SPORTS GROUND, ENFIELD
CLIENT	ENFIELD COUNCIL
ARCHITECT	MCLAREN ASSOCIATES RESIDENTIAL AND DESIGN SERVICES 10001/020/030/040/050/060/070/080/090/100/110/120/130/140/150/160/170/180/190/200/210/220/230/240/250/260/270/280/290/300/310/320/330/340/350/360/370/380/390/400/410/420/430/440/450/460/470/480/490/500/510/520/530/540/550/560/570/580/590/600/610/620/630/640/650/660/670/680/690/700/710/720/730/740/750/760/770/780/790/800/810/820/830/840/850/860/870/880/890/900/910/920/930/940/950/960/970/980/990/1000
SCALE	1:100 (A3) / 1:500 (A4)
DRAWN BY	DP
CHECKED BY	
DATE	JUNE 2015
PROJECT NO.	858
DRAWING NO.	107

FOR PLANNING

SECTION A-A THROUGH LIGHTING MAST THROUGH EASTERN PART OF SITE MAIN TYPICAL PITCH MCGA

red line = proposed finish
green line = existing level

SECTION AA - THROUGH LIGHTING MAST THROUGH EASTERN PART OF SITE MAIN TYPICAL PITCH MCGA

SECTION BB - THROUGH LIGHTING MAST THROUGH EASTERN PART OF SITE MAIN TYPICAL PITCH MCGA

SECTION CC - THROUGH LIGHTING MAST THROUGH EASTERN PART OF SITE MAIN TYPICAL PITCH MCGA

SECTION DD - THROUGH LIGHTING MAST THROUGH EASTERN PART OF SITE MAIN TYPICAL PITCH MCGA

SECTION EE - THROUGH LIGHTING MAST THROUGH EASTERN PART OF SITE MAIN TYPICAL PITCH MCGA

SECTION FF - THROUGH LIGHTING MAST THROUGH EASTERN PART OF SITE MAIN TYPICAL PITCH MCGA

SECTION GG - THROUGH LIGHTING MAST THROUGH EASTERN PART OF SITE MAIN TYPICAL PITCH MCGA

SECTION HH - THROUGH LIGHTING MAST THROUGH EASTERN PART OF SITE MAIN TYPICAL PITCH MCGA

SECTION II - THROUGH LIGHTING MAST THROUGH EASTERN PART OF SITE MAIN TYPICAL PITCH MCGA

SECTION JJ - THROUGH LIGHTING MAST THROUGH EASTERN PART OF SITE MAIN TYPICAL PITCH MCGA